



Government of Jersey

HAVRE DES PAS PEDESTRIAN CROSSINGS

Stage 1 & 2 Road Safety Audit (Rev A)





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Stage 1 & 2 Road Safety Audit (Rev A)

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WSP

The Forum
Barnfield Road
Exeter, Devon
EX1 1QR

Phone: +44 1392 229 700

Fax: +44 1392 229 701

WSP.com



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1. INTRODUCTION

1.1. GENERAL

1.1.1. WSP has been commissioned by [REDACTED], Government of Jersey (Department for Infrastructure, Housing and Environment), to carry out a Stage 1 & 2 Road Safety Audit on the proposed raised pedestrian crossings along Havre des Pas, St Helier.

1.1.2. The road safety audit team membership was the following:

[REDACTED] Road Safety Audit Team Leader
Associate
WSP

[REDACTED] Road Safety Audit Team Member
Associate
WSP

The Audit Team was approved by [REDACTED], the Audit Sponsor.

1.1.3. This audit took place on site during November 2021, and the site was examined by [REDACTED] and [REDACTED] together during daylight hours between 10:00 and 11:30 on 10th November 2021. The site was also visited by [REDACTED] and [REDACTED] on the same day during the hours of darkness between 21:00 and 21:15.

1.1.4. The weather during the site visit was overcast and the carriageway surface was dry.

1.1.5. The Road Safety Audit also comprised an examination of the documents and drawings supplied to the Road Safety Audit Team, referenced in Appendix A of this report. The location of problems raised can be found in Appendix B.

1.1.6. The terms of reference for the audit are as described in the Government of Jersey Road Safety Audit Policy.

1.1.7. The Road Safety Audit Team has examined and reported only on the road safety implications of the scheme as presented and has not examined or verified the compliance of the designs to any other criteria. This Road Safety Audit has not considered structural safety or checked for compliance to standards. This Road Safety Audit has been undertaken based on the Road Safety Audit Team's previous experience and knowledge in undertaking Accident Investigation, Road Safety Engineering and Road Safety Audit. No member of the Road Safety Audit Team has had any previous input to the design of the scheme.

1.1.8. Accident data (2015 – 2019) provided for this Road Safety Audit shows that there were 23 road traffic collisions within this 5-year period, 3 of which were serious and 20 were slight. 34% of vehicles involved in the collisions were motorcycles (40% of the casualties) and 18% were pedal cycles (26% of the casualties). Pedestrians accounted for 11% of the casualties. 8 of the 23 collisions occurred in August.

1.1.9. No previous Road Safety Audits have been carried out for this scheme.

- 1.1.10. Since the issue of the original Stage 1 & 2 Road Safety Audit Report, the scheme has been altered and the Safety Audit Team have been requested to revise the report to reflect these changes. In particular, the number of raised table courtesy crossings have been reduced from 7 to 3. The raised table outside the Ommaroo Hotel has also been amended to accommodate a zebra crossing.
- 1.1.11. This updated Road Safety Audit Report can only comment on the proposals currently being put forward and their relative merits compared to the original road layout. The Road Safety Audit Team are unable to comment on the merits of these latest proposals compared to any alternative layouts previously developed for consultation.

1.2. PURPOSE OF SCHEME

- 1.2.1. The scheme comprises of two-lane single carriageway and forms part of the coast road into / through St Helier from the east of the Island. It is a pedestrian heavy area with access to the seafront and numerous tourist-centric businesses.

The aim of the scheme is to improve the driveability of the road with improved carriageway quality, as well as improved pedestrian safety with the inclusion of 3 no raised tables and a new zebra crossing outside the Ommaroo Hotel.

- 1.2.2. The proposed works included:

- § removal and replacement of the existing pavement
- § installation of 3 no raised tables with associated drainage, road marking, kerb and footway works
- § relocation / alteration of traffic signals and installation of astucia studs
- § build out kerblines around the western side of the bellmouth into Roseville Street

- 1.2.3. At the time of the original road safety audit the scheme comprised the installation of seven raised table crossings. The designer indicated that, subject to the outcome of ongoing consultations, some of the crossings may be omitted from the final scheme to be taken forward. This situation has subsequently materialised, and the number of crossings has been reduced to three.
- 1.2.4. Additional general comments / explanations are included in Appendix C.

2. PROBLEMS IDENTIFIED AT THIS STAGE 1 & 2 ROAD SAFETY AUDIT

2.1. LOCAL ALIGNMENT

No problems of this type were identified during this audit.

2.2. GENERAL

No problems of this type were identified during this audit.

2.3. JUNCTIONS

No problems of this type were identified during this audit.

2.4. WALKING, CYCLING AND HORSE RIDING

No problems of this type were identified during this audit.

2.5. TRAFFIC SIGNS, CARRIAGEWAY MARKINGS AND LIGHTING

No problems of this type were identified during this audit.

No additional Problems have been identified in this Revision of the Road Safety Audit Report

End of Problems identified and Recommendations offered in this Stage 1 & 2 Audit

3. AUDIT TEAM STATEMENT

We certify that this audit has been carried out in accordance with the Government of Jersey Road Safety Audit Policy.

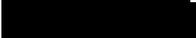
AUDIT TEAM LEADER


Associate

Signed: 

WSP
The Forum
Barnfield Rd
Exeter
EX1 1QR

Date: 07/02/22

Tel: 
Mob: 

Appendix A



DOCUMENT LIST

List of documents and plans considered during this Stage 1 & 2 Road Safety Audit:

Road Safety Audit Brief (Form B)

Drawings - General

1964-0100-001 A	Scheme extents and Site Location
1964-0100-101	Core Location Plan Sheet 1 / 2
1964-0100-102	Core Location Plan Sheet 2 / 2
1964-0200-001	Site Clearance Sheet 1/2
1964-0200-002	Site Clearance Sheet 2/2
1964-0500-001 E	Works to Existing Chambers & Proposed Drainage Sheet 1/2
1964-0500-002 E	Works to Existing Chambers & Proposed Drainage Sheet 2/2
1964-0700-001 C	Pavement Details Sheet 1/2
1964-0700-002 C	Pavement Details Sheet 2/2
1964-1100-001 D	Kerbs and Footways Sheet 1/2
1964-1100-002 D	Kerbs and Footways Sheet 2/2
1964-1200-001 B	Road Marking Details Sheet 1/2
1964-1200-002 B	Road Marking Details Sheet 2/2
1964-1400-001 A	Service Ducts
1964-SD-001 B	Standard Details
1964-SD-001 B	Standard Details
1964-SD-001 A	Standard Details
1964-SK-006	Raised Table 3 – Crossing Studs

RTC_HDP Summary

Collision Data Summary

Appendix B

PROBLEM LOCATION PLAN

WSP

**Problem Location Plan
Stage 1 & 2 Road Safety Audit**

Comment 8 & 9 -
Area Wide

Comments 1, 4,
5, 6 & 7 - No
Longer Valid

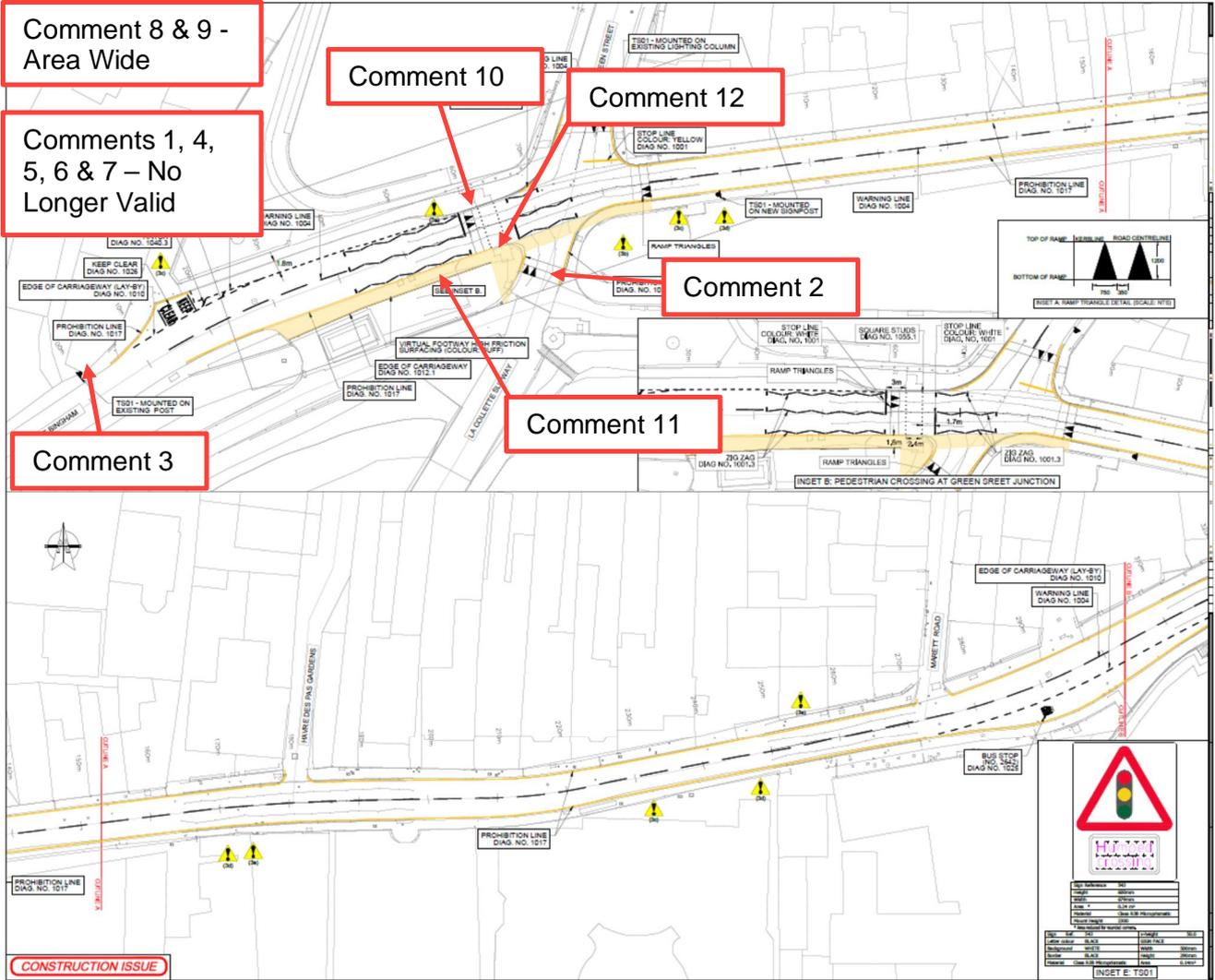
Comment 3

Comment 10

Comment 12

Comment 2

Comment 11



Appendix C

GENERAL COMMENTS



GENERAL DESIGN COMMENTS (INC NON-SAFETY ITEMS)

C1 COMMENT 1

Green Street Junction – the mapping used on the drawings for this junction is out of date. In particular, the kerbed build out on the southern side of the existing pelican crossing extends further to the west than shown. The extended buildout will assist with the tie in of levels for the ramp on the western side of the raised table. Other details at this location should be checked to ensure that the design is compatible with the new layout. **Mapping appears to have been updated in the revised drawings.**

C2 COMMENT 2

Green Street Junction – no level details or sections have been provided for the raised table at this junction. The drawings show a ramp on the southern approach to the table, which starts a few metres in front of a cobbled section of slipway and close to the seafront promenade. A ramp at this location could cause an unnecessary hazard for pedestrians as well as be difficult to tie in to on its eastern side (where the adjacent levels are flush). The need for a ramp at this location is unclear as approach speeds will be very low. It is recommended that the levels on this southern side of the junction are checked to ensure that an appropriate solution is achieved. **Comment still valid in this revision of the RSA Report, although it is understood that level checks have been made.**

C3 COMMENT 3

Western approach to the Green Street Junction – traffic signs TS01 & TS02 are shown in close proximity to each other. Although not considered to be a problem, it is recommended that checks are made to ensure that the larger sign (TS02) does not obstruct visibility to the 20mph sign (TS01) located behind it. **Comment still valid in this revision of the RSA Report, although it is understood that appropriate checks have been made.**

C4 COMMENT 4

Raised table adjacent to the Fort D’Auvergne Hotel – at the time of the site visit there was considerable construction work being undertaken at the hotel site. It is not known whether these developments will include any vehicular access that could conflict with the location of the proposed raised table. **Comment not valid in this revision of the RSA Report – raised table removed.**

C5 COMMENT 5

Pedestrian Crossing at the Lido – the stop lines are shown positioned directly at the base of the ramps to the raised table. This may make the lines less noticeable to drivers as well as cause vehicles to roll back if they slightly overshoot the stop line. It is recommended that the lines are positioned at a suitable distance back from the ramps. **Comment not valid in this revision of the RSA Report – road markings adjusted.**

C6 COMMENT 6

Raised Table opposite the Ommaroo Hotel – there is an existing dropped crossing with tactile paving a few metres to the west of the proposed raised crossing. There are no plans to remove this crossing, however, having two crossings in such close proximity is quite unusual. **Comment not valid in this revision of the RSA Report – kerb line has been amended.**

C7 COMMENT 7

Raised Pedestrian Crossing near the junction with St Clement's Road – unlike the other crossings, this raised table is located on a section of road between streetlights. Although no obvious dark areas were observed during the night visit, it may be necessary to check lighting levels in this area to ensure that the raised table is clearly visible to approaching drivers. **Comment not valid in this revision of the RSA Report – raised table removed.**

C8 COMMENT 8

Area Wide – 20mph signs are to be installed at either end of Havre des Pas, which is within an existing 20mph limit. The audit team did not check to see whether the 20mph limit is also signed in each side road. The validity of the 20mph speed limit could be questioned if it is not signed on the side roads joining it. **The 20 mph signs previously proposed for Havre des Pas have been omitted in from the latest version of the drawings. Check that adequate repeater signs are provided along the length of Havre des Pas.**

In addition, the Highways (Road Humps) (Jersey) Regulations 2002 requires that all road humps are signed with traffic sign No. 557.1 in combination with traffic sign No. 557.2, 557.3 or 557.4. It is noted that where the speed humps are within a 20mph the Highway Authority, in consultation with the Minister, may decide that these signs are not required. **Comment still valid in this revision of the RSA Report.**

Further Comments on Amended Scheme:

C9 COMMENT 9

There are a few anomalies with the road markings shown on the current version of drawings 1964/1200/001 & 1964/1200/002.

Green St Junction & the Crossing near Roseville Junction - The central zig zag markings on the eastern side are the wrong way around. On the approaches to the crossing the zig zag markings should run in parallel, with the marks converging and diverging on the exit side.

Crossing near Roseville Junction - The zig zags are not in pairs (ie an odd number, not even). This means that the last mark does not tie into the kerb. Either extend or reduce by one mark.

Zebra crossing outside Ommaroo Hotel – Check the configuration of the striped markings to ensure that the first white stripe is away from the kerb face.

C10 COMMENT 10

Green St Junction – the tactile paving at the signalised crossing doesn't appear to line up correctly.

C11 COMMENT 11

Green St Junction – high friction surfacing has been proposed for the virtual footway on the southern side of the junction. Pedestrians that trip and fall on to the surface may sustain more severe injuries than if a less abrasive surfacing material is used.

C12 COMMENT 12

Green St Junction – buff coloured high friction surfacing has been proposed for the virtual footway on the southern side of the junction. Buff coloured tactile paving has also been proposed at the crossing and therefore it is important to provide adequate contrast for visually impaired pedestrians.

The Forum
Barnfield Road
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EX1 1QR

wsp.com