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THE TOWN OF ST HELIER

PROPOSALS FOR THE RING ROAD - ST SAVIOUR'S ROAD

18th December, 1985

Negotiations having been opened between the Department of Public Building and Works and the Jersey Gas Company on the alignment of the proposed link road between St Saviour's Road and Oxford Road, it remained to seek the views of C. Le Masurier Ltd. This was done at a meetings between the Planning Adviser and [REDACTED] the first of which took place on 22nd August. It was agreed that there were three main options, briefly described below, and clearly understood that Le Masuriers would be unwilling to sell any of the Company's land.

OPTION 1 - TO DO NOTHING

MAP No.1

This is the option preferred by Le Masuriers. From the public viewpoint it has a number of disadvantages, the main ones being:

- It is not possible to increase the capacity of the Ring Road between Stopford Road and Tunnell Street, nor to easily improve the junctions, especially that at the foot of Wellington Road. The Public Works Committee has already asked the States Land and Property Officer to negotiate the sale of land at Bellville in case the recommendations of this report are not accepted.
- Access to the proposed housing developments round Westley Lodge and at the Animals' Shelter would be difficult and expensive to provide, the only possible solutions leading to a worsening of junction difficulties and a further reduction in the capacity of the Ring Road.
- Access to the proposed Oxford Road multi storey car park would have to be via the existing streets, an indirect and low-capacity route which would increase the congestion at the junctions of Stopford Road and Tunnell Street with St Saviour's Road. The full potential of the car park could not be realised if its capacity has to be geared to the ability of the access roads to carry peak hour traffic.

The problems of securing title to some of the properties that form the 'Westley Lodge Housing Development', and coming to satisfactory arrangements with owners who enjoy rights of way over the site, may never be adequately solved.

Map No. 1 shows the main owners of land in the area.

A. The Jersey Gas Company Ltd.

Once the production works have moved to La Collette the Company's activities in town are concentrated on this site.

B. The Public of the Island

Bought from the Gas Company some years ago.

C. C. Le Masurier Ltd.

Comprises land acquired from the Gas Company some years ago (northern part), this year (southern part) and from other owners at various times (east of the brook). Rights of way to St Saviour's Road and to the brook are not shown.

D. The Public of the Island

Acquired from the Gas Company and others in order to assemble a site on which the Housing Committee can build flats for rent. A property on Stopford Road, needed to gain access to the site, has yet to be acquired as has the cottage and garden at the southern extremity, but they are included on this map. The title to some of the properties at the southern end of the site is difficult to establish and the situation is complicated by ill-defined rights of way to St Saviours Road, to a 'common' and to a well held by adjoining owners.

The extreme western part of the site of the Mimosa Hotel will also be needed. The President of the Public Works Committee reports that he has informed the owners.



D

C

A

B

Woolley Lodge

Church

School

Gas Holder

Gas Works

Tanks

Car Park

Animals' Shelter

KINGSLEY HOTEL

PH

PH

ES

RONSLEY LANE

STOPFORD ROAD

OXFORD ROAD

CHEVALIER ROAD

COMMON LANE

BEAULIE ROAD

ROAD

STREET

PLACE

40.0+

36.0+

37.0

43.0

46.0

44.0

50

40.0

+ 33.0

32.0+

36.0

+ 34.0

31.0

OPTION 2 - RECOMMENDED ROAD CONSTRUCTION

MAP No.2

This is the option recommended by the Planning Adviser and Manager, Capital Works, of the Department of Public Building and Works. It is the option that causes most disturbance to Le Masuriers. The advantages to the public do not need describing here other than to confirm that the capacity of the Ring Road would be increased, junction traffic movements eased and adequate access to the proposed multi storey car park gained. Though costly, the benefits are great and far-reaching and partly off-set by the fact that improvements to junctions, necessary if Option 1 was to be adopted, do not have to be carried out.

The land necessary to construct the roads has to be acquired from the Jersey Gas Company, Le Masuriers and the Mimosa Hotel. It has been suggested that if Le Masuriers is forced to give up land it would be more interested in receiving an equivalent area in exchange. This would be taken from that assembled by the public for housing development purposes. The area required to create the proposed roads, shown diagonally hatched on the map, is some 2620 square metres (28200 square feet). If the States housing proposals are not to be seriously compromised, only about 540 square metres (5800 square feet) could be made available in exchange. This area is shown vertically hatched on the map and will be seen to exclude part of the lane over which both parties, and others, have rights of way and whose ownership is unclear. For traffic reasons it would be wise to extinguish any vehicular rights of way to St Saviours Road and replace them with rights of access to the proposed new road instead.

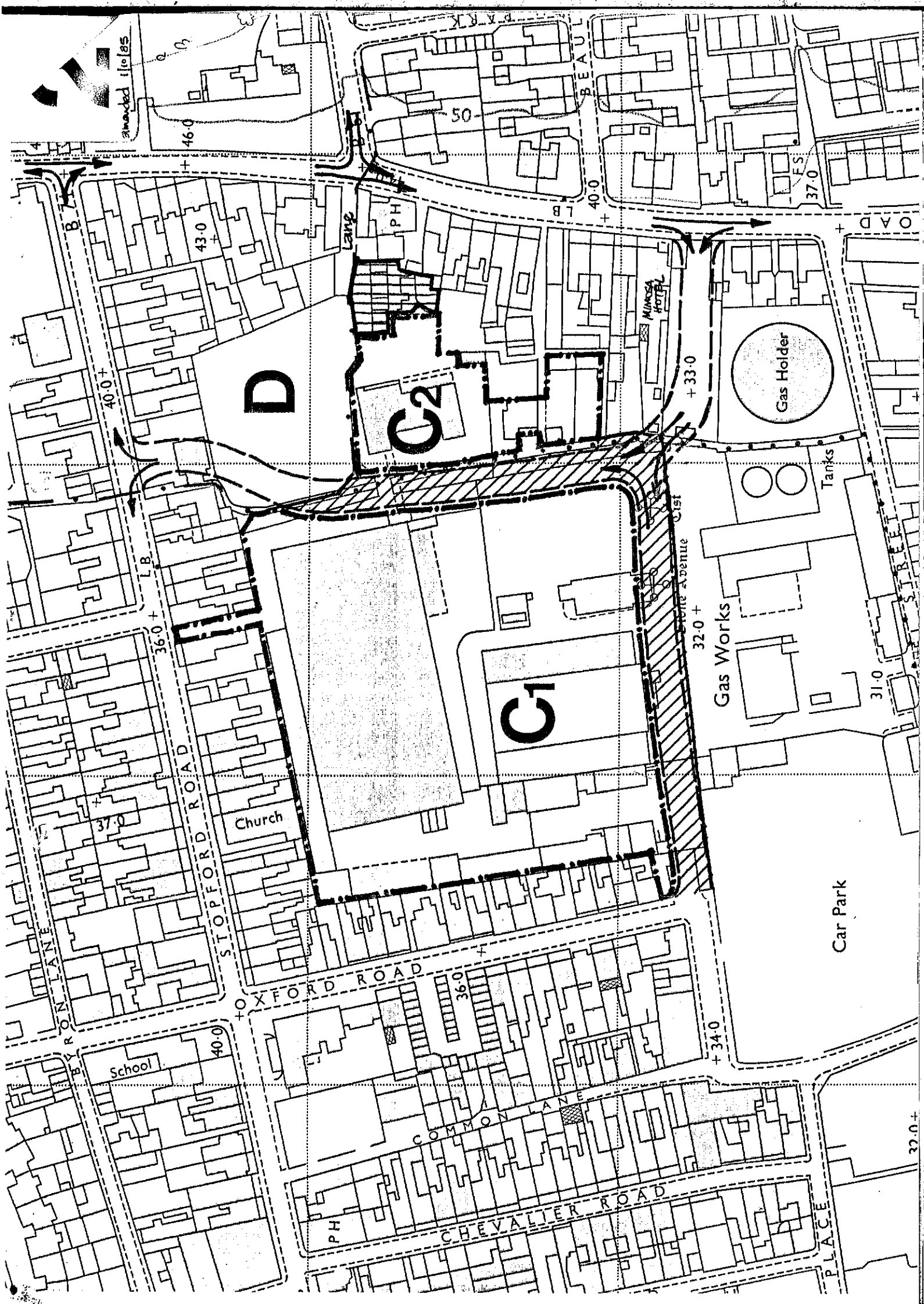
Also needed is a small area of the rear garden of the Mimosa Hotel. An equivalent area to the south of the hotel could be made available in compensation and the hotel would also gain a valuable new road frontage.

In addition to the land transactions, the public will be required to compensate Le Masurier's for the effect the road proposals will have on the Company's existing business and on plans for its extension. Compensation could take the form of accommodation works provided by the public, the main ones being:

- A security wall on the new boundary of Le Masurier's site formed by the proposed roads.
- Arrangements to provide access to existing buildings and to rebuild parts of buildings demolished to make way for the roads.
- A replacement pedestrian bridge at second floor level the link the existing warehouse and office buildings.

Against these and other works to accommodate the Company may be set off the advantages gained by the creation of new road frontages and the opportunity to form new access points to the site; features that could increase the value of the Company's site and increase its development potential.

Accommodation works required by the Jersey Gas Company include the construction of a wall on the boundary between the new road and the Company's land together with an access gateway at a point yet to be determined.



OPTION 3 - MODIFIED ROAD ALIGNMENT

MAP No.3

This option has been suggested by C. Le Masurier Ltd. as a means of reducing the disturbance to the Company's property. The realignment of the proposed northbound Ring Road section avoids the division of Le Masurier's property brought about by the adoption of Option 2 and reduces the loss of amenity and security which would be caused by the preferred alignment.

Unfortunately, among less important disadvantages, the States housing development proposals are seriously affected and a number of properties would have to be acquired. The former difficulty would add to the cost of building new dwellings while the latter could be expensive, difficult and environmentally damaging to residents of residential properties adjoining the road.

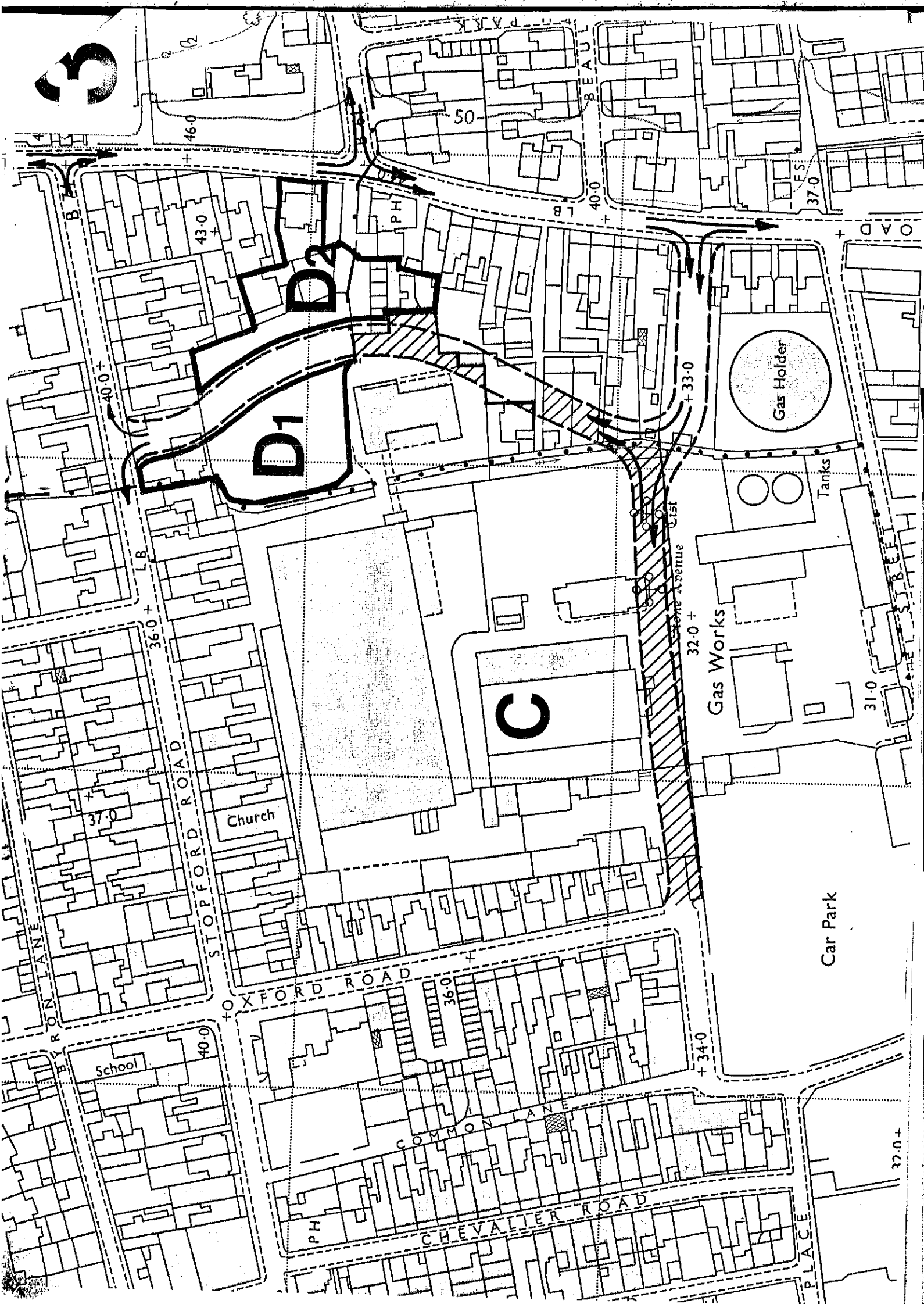
DECISION

The Planning Department and Department of Public Building and Works recommended that the road proposals outlined in Option 2 be adopted. The Island Development Committee agreed and decided to include it in the Island Plan.

Because both the Housing Committee and Le Masuriers wish to press ahead with plans for the development of their sites it was further recommended that informal negotiations and discussions continue in anticipation of the States approving the proposals.



PLANNING ADVISER



D1

Church

C

Gas Works

Gas Holder

Tanks

Car Park

